



Town of Brookeville Comprehensive Plan

Update to Transportation, Pedestrian Networks & Streetscaping Elements

Brookeville, Maryland



LARDNER/KLEIN
LANDSCAPE ARCHITECTS, PC

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Lardner/Klein Landscape Architects, P.C.

Town of Brookeville Comprehensive Plan
**Update to Transportation,
Pedestrian Networks &
Streetscaping Elements**
Brookeville, Maryland

7 September 2021

Acknowledgements

The Brookeville Comprehensive Plan Update to Transportation, Pedestrian Networks and Streetscaping Elements was initiated in March of 2021 and completed over a six-month period. The plan was prepared with the input and contributions of many residents and stakeholders taking the time to participate and provide ideas and suggestions, as well as thoughtful comments on the plan. Input to the plan update was provided through a community walking tour, an initial public meeting held virtually, and an in-person public meeting to review the initial draft of the plan. Many participants as well as the following commission members and staff contributed to the development of the plan:

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Bill Gaskill, President (chosen by Commission) term through 2022

Garrett Anderson, term through 2021

Mark Davis, term through 2021

Planning Commission Members

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Miche Booz (2021)

Margaret Kay (2022)

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Purpose of the Plan Amendment

Two substantive events have led to the need for an update to the Transportation, Pedestrian Networks & Streetscaping Elements of the Town of Brookeville Comprehensive Plan:

- First, after more than 50 years of work with local, state, and federal agencies, the Brookeville Bypass is nearing completion. Once open to the public, the Byapss will eliminate more than 10,000 vehicles per day that currently pass through the Town, allowing for High St. and West Market St. (now MD 97) to be designated as a local street instead of a minor arterial highway. Designation as a local street has the potential to allow for narrower vehicle travel lanes and lower speed limits and thereby increasing pedestrian safety and access with less impact from right-of-way acquisitions and substantial regrading and retaining walls that otherwise would be required.
- Second, the Town has received a State of Maryland grant to help fund improvements to the Town-maintained portion of Market Street (referred to as East Market St.). Before making any substantive improvements to the road, the Town would like to explore alternative road configurations that provide for traffic calming and streetscape changes that enhance the aesthetics of the Town while maintaining its rural, village character.

The Town's inclusion on the National Register of Historic Places in 1979 and 1986 designation as a local Historic District, subject to the Historic Preservation Ordinance, Chapter 24A of the Montgomery County Code, adopted in 1985 by the Town Commissioners, serves to protect its integrity as a relatively unaltered 19th century rural town by providing enforceable historic preservation requirements for property owners. The plan amendment also offers an opportunity for the Town to update their goals that originally envisioned the Byapss and to reclaim their streets for local traffic and pedestrians in a manner consistent with its listing on the National Register and designation as a local Historic District.

According to the 2010 Comprehensive Plan:

"The Brookeville Comprehensive Plan was established by the Town in the year celebrating the Bicentennial of its founding, 1994, and as subsequently revised, proactively seeks through the planning process to preserve Brookeville's existing buildings, natural features, open space, and density.

The importance to the community of preserving its heritage is evident in the Town's initially adopted goals and objectives. Although these goals address a number of social and community issues, they reflect an overriding concern that the community response be appropriate and sensitive to its stewardship of this historic town."

With the Byapss ready to open in 2021, the Town can develop more context sensitive streets and sidewalks that serve to reinforce the preservation goals. The following objectives under Goal #1 are revised to reflect the changes in status of the Byapss and its and related opportunities:

Goal # 1**Preserve and enhance Brookeville's historic rural village character**

Change the third objective to say:

Apply cultural conservation objectives and design criteria in the execution of Town capital improvements such as “streetscaping.” Coordinate with the State and County to develop a transportation plan that will implement the planned bypass while retaining the Town’s accessibility to local traffic and implement plans that recapture High and Market Street as local streets, reconnect a network of sidewalks and trails, and restore Brookeville’s public streetscape and its historic sense of place.

Change the fourth objective to say:

Work with the State and County to identify and implement appropriate road improvements that would ease current traffic conditions, slow vehicular operating speeds through town and increase pedestrian and bicycle safety in a context sensitive manner.

Add the following paragraph at the end of the section “**Developing the Comprehensive Plan**”:

In 2021, with the completion of the Bypass imminent, the Town hired Lardner/Klein Landscape Architects to work with the Planning Commission, Town Commissioners, and the residents of Brookeville to develop overall concepts for slowing vehicular operating speeds, reconnecting a network of sidewalks and trails, and restoring Brookeville’s public streetscape and its historic sense of place. Concepts were developed through a public walking tour, an on-line questionnaire, and a virtual public brainstorming session. The concepts were then refined and reviewed at an in-person public meeting, all conducted over a period of three months. The resulting plan amendment reflects both the expressed needs and desires of the community, as well as the importance of preserving and enhancing Brookeville’s rural and historic character.

Revise the last sentence of the paragraph of the section “**Transportation Elements**” to:

However, the Town’s main objective resides in the implementation of the taking best advantage of the opportunities created with the 2021 opening of the Brookeville Bypass. The opening of the Bypass will allow the Town to preserve its historic character as well as provide opportunities for increased pedestrian and non-automobile links, slowing vehicular operating speeds, reconnecting a network of sidewalks and trails, restoring Brookeville’s public streetscape and its historic sense of place, and further enhancing non-motorized linkages to areas south along Route 97, such as Olney.

Public Transportation Network

Replace the entire section with the following:

Public transportation in Montgomery County is provided by the Washington Metropolitan Area Transit Authority (WMATA) and the Montgomery County Ride On system. WMATA operates Metrorail transit and bus service across the Washington region, while Ride On operates bus service within Montgomery County. However, neither transit operator directly serves Brookeville. The closest Ride On bus stop is at Gold Mine Road and Dubarry Lane, nearly a half mile beyond the southern town limit. That stop is served by the 53 bus. This bus line connects the Olney area to both the Shady Grove and Glenmont Metro Red Line stops.

Pages 13-20

Replace sections

Georgia Avenue - Brookeville Bypass

Management of East-West Traffic

Impact of the Inter County Connector

Interim Traffic Improvements

Town Circulation System

with the following pages

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Town Transportation Network

With the opening of the Brookeville Bypass, the Town's transportation network will be dramatically changed from one where MD 97, Georgia Avenue, dominated and divided the historic town to a more appropriately scaled and traveled network of local streets—High Street, East and West Market Street, and its town-maintained streets (Figure 1).

Existing Conditions

Maryland 97 (MD 97) is known as Georgia Avenue outside the limits of the Town of Brookeville. Within the Town it is known as High Street and West Market Streets. Maryland 97's current functional classification is "Minor Arterial." MD97 is within the federally classified "Washington, DC--VA--MD Urbanized Area" with implications for funding eligibility. The Maryland Department of Transportation is currently responsible for the maintenance of MD 97.

Brookeville is within the jurisdiction of the National Capital Region Transportation Planning Board, the Metropolitan Planning Organization (MPO) that serves Montgomery County.

The Town of Brookeville is responsible for the maintenance of its local streets. Town rights-of-way include East Market Street, North Street, South Street, Water Street, a portion of Race Street and a portion of Church Street. The Town also owns right-of-way for unpaved streets (referred to as "paper streets") including Back Street, and Spring Alley.

Historic Significance

The listing of the Town of Brookeville on the National Register of Historic Places (NRHP) and its designation as a local Historic District in Montgomery County ensures that any modifications to public rights-of-way will require a Historic Area Work Permit from the Montgomery County Historic Preservation Commission. If federal funding or approval is involved, Section 106 of the National Historic Preservation Act of 1966 (NHPA) requires federal agencies to consider the effects of projects they carry out, approve, or fund on historic properties. Section 106 review ensures that preservation values are factored into federal agency planning and decisions.

One of the key factors in determining whether a project is consistent with Section 106 and is often used by local historic

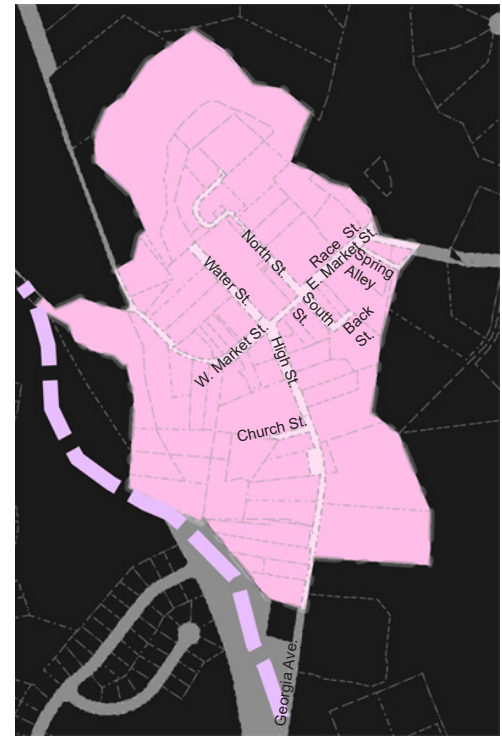


Figure 1 - Existing public rights-of-way in Brookeville

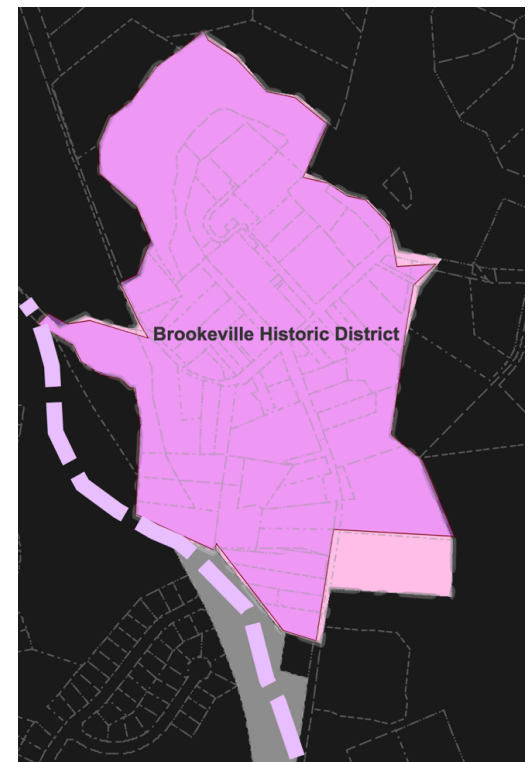


Figure 2 - Brookeville Historic District Boundary

district reviews, is the statement of significance that led to a listing on the NRHP or designation as a local Historic District. The Brookeville Historic District's nomination includes the following statement of significance:

"Brookeville is a unique town in Montgomery County because of its collection of unaltered early and mid-19th century architecture and its pristine setting. Montgomery County is under great development pressure, and new development has encroached upon many of the county's earlier towns, e.g., nearby Olney. Brookeville remains untouched, with its narrow, curving streets and great trees. The houses represent most periods of architecture from the late 18th century to the 20th, but predominantly the Federal and mid-19th century, with Victorian touches. These are typical examples of these styles, in largely unaltered condition."

Character of Existing Roadways

The Town's character defining features provide another basis for developing "context sensitive solutions" for modifications to the transportation network as envisioned in this plan amendment.

The character defining features associated with each of the Town's public streets is different, depending upon the period of significance of its contributing resources. The nominating document for Brookeville's listing in the National Register of Historic Places identifies the periods of significance, which has been recreated in Figure 3. For those areas identified as primary resources, any transportation related enhancements will need to fit within that historic context, and to the extent practical, reduce contrast through the selection of appropriate materials for curb and gutter, sidewalks, crosswalks, lighting, and related infrastructure.

The secondary resources noted as "Post 1940" in the southern part of town may provide the greatest opportunity for inserting measures to slow traffic that rely upon high visual contrast to capture the attention of drivers and encourage them to slow down (such as splitter islands, pinch points, and other measures that change the horizontal alignment of the roadway).

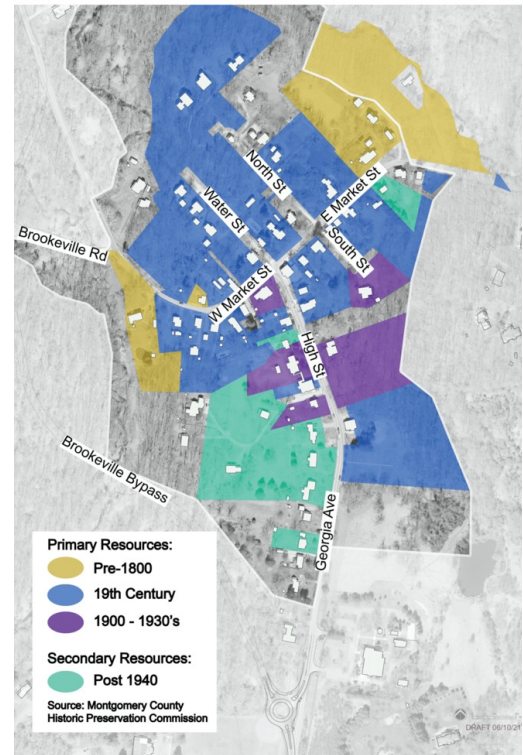


Figure 3 - Periods of Significance



Figure 4 - Distinctive 19th Century character of High Street at the Brookeville Academy



Figure 5 - Post 1940's character of the southern end of town

Sidewalks and Pathways

Figure 6 illustrates the locations of existing sidewalks and pathways and the materials that are used to construct them (brick concrete, or asphalt).

Brick sidewalks are located along both sides of East Market Street and on the north side of West Market extending west of the High Street/East Market Street intersection. Brick sidewalks extend on the east side of High Street and then switching over to the west side in front of the Salem United Methodist Church. The Church parking lot also serves as an informal overflow parking lots for events at the Brookeville Academy, but no marked crosswalk is available between the two walkways. A concrete sidewalk is found on the southeast corner of High and Market Street.

A shared use asphalt path is found on private property extending along the east side of High Street south of the Salem United Methodist Church cemetery along the frontage of the Manor at Silo Falls.

There is a formal walking path connecting North and Water Streets on Town of Brookeville property. Informal walking paths are utilized by many residents that connect to adjacent parkland.

Utilities

A major factor in the restoration of Brookeville's public streetscape and its historic sense of place is the presence of overhead electric utility lines as shown in Figure 7. The utility lines dominate the view along Georgia Avenue approaching town and limit the ability to plant street trees in the space available. Certain kinds of trees can be planted under overhead electric lines, but they are limited in size and growth characteristics.

Projected Traffic Volumes Post Bypass

As the basis for the County's approval of the project, with conditions, Table 1 from the "MD97 Brookeville Bypass, Mandatory Referral No. 2016030" (prepared by Montgomery County in 2016), noted a shift in forecasted volumes to the Bypass (13,025) from High Street (down to 900) and Market Street (down to 600) with current volumes on the left in vehicles per day.

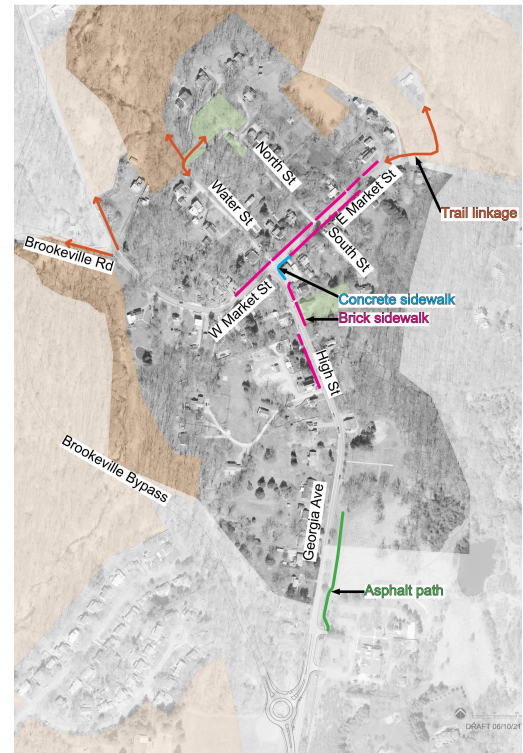


Figure 6 - Existing sidewalks and pathways

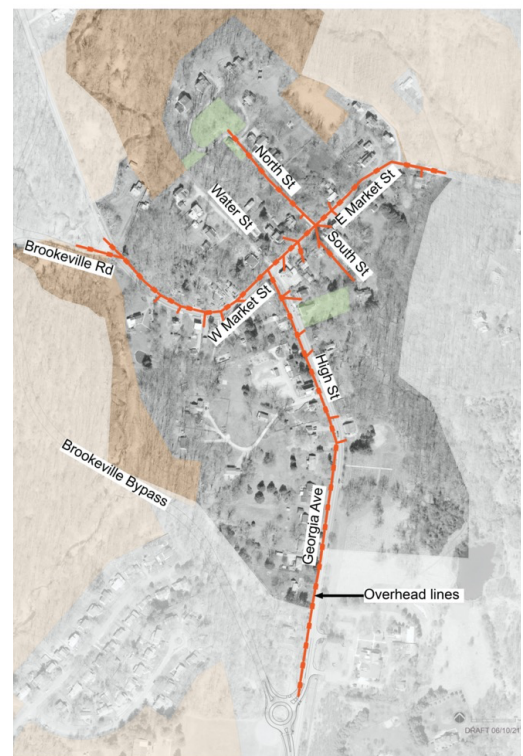


Figure 7 - Existing overhead electric lines

Traffic Volumes (vehicles per day) Road Name	Existing Traffic Volume	Forecast 2040 Volume for No- Build Condition	Forecast 2040 Volume for Alt 7M Adjusted (Proposed Project)	Forecast 2040 Volume for Alt 8B Adjusted (Long Bridge Alternative)
High Street	10,400	13,725	900 (a 93% reduction)	1,225 (a 91% reduction)
Market Street (between High Street and Georgia Avenue)	10,275	13,575	600 (a 96% reduction)	925 (a 93% reduction)
MD97 Bypass (south of Brookeville Road)	0	0	13,025	12,700

Table 1 - Project traffic volumes from

https://montgomeryplanningboard.org/agenda/2016/documents/MD97BrookevilleBypassMandatoryReferral2016030_staffreport.pdf accessed on April 26, 2021

The significant reduction in traffic volumes anticipated when the Bypass opens for traffic will allow the Town to request that the Functional Classification be changed from “Minor Arterial” to “Local” serving the needs of the community, rather than for through travel. In addition, the design standards for streets with lower traffic volumes are typically reduced to match those volumes. Streets with lower traffic volumes, but designed for higher volumes, often result in higher operating speeds when traffic capacity opens. Traffic calming measures will be needed to reduce higher operating speeds and better match local street functions (rather than through travel).

MDOT SHA Sidewalk Concept

MDOT SHA prepared a sidewalk plan to extend the sidewalk from the southern terminus of the Bypass (a roundabout) to the existing sidewalk at the Salem United Methodist Church. The sidewalk plans also included existing shoulders to address MDOT’s bicycle policy which requires that accommodation for bicycles be incorporated into all state projects. The policy states, “No project shall reduce an existing shoulder width to less than the mandatory conditions of these guidelines on roadways where bicycle are permitted without an approved bicycle design waiver.” Should MD 97 continue to be maintained as a Maryland State

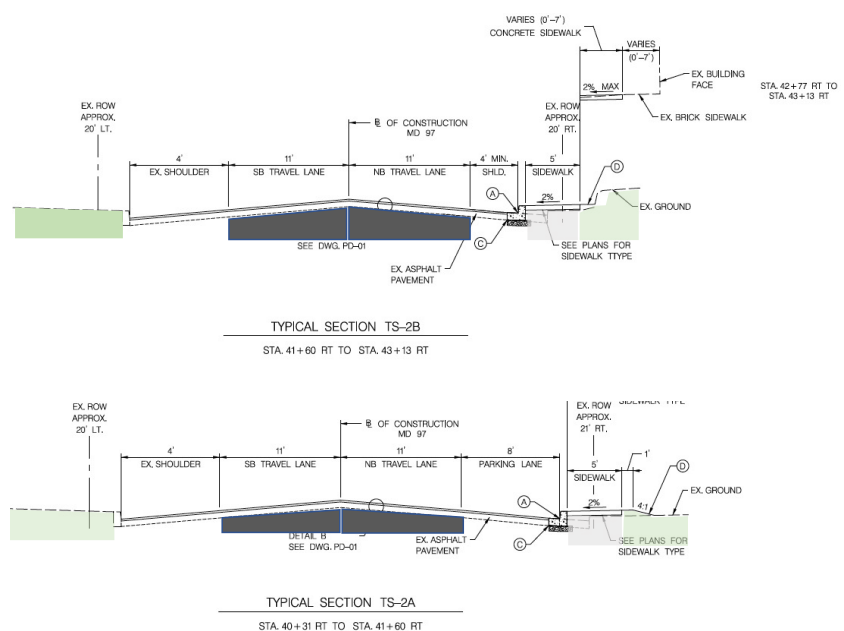


Figure 8 MDOT SHA sidewalk concept typical sections (May 2018)

Highway, then a waiver will be required to reduce the shoulder widths to accommodate sidewalks on both sides of the street within the existing right-of-way.

Community Vision

As part of the planning process, residents responded to an online questionnaire to gain a better understanding of preferences and priorities. About half of the households responded to the questionnaire. The full results are included in Appendix 1. Figure 9 depicts the most utilized words from respondents when asked to describe what they liked best about Brookeville and what are the streetscapes most distinguishing characteristics.

Respondents identified the following as the most frequent daily use patterns:

- I drive through Brookeville to run errands.
- I drive through Brookeville to get to other destinations.
- I use it to drive to work.
- I walk or jog in Brookeville for exercise or enjoyment.

Respondents identified the following as the features that they would most like to see changed:

- Create more sidewalks
- Narrow travel lanes and use space for sidewalks or trees
- Bury utility wires to reduce visual impact.
- Insert traffic calming measures to slow operating speeds.



Figure 9 - Most used words to describe what respondents liked best about Brookeville and its most distinguishing characteristics

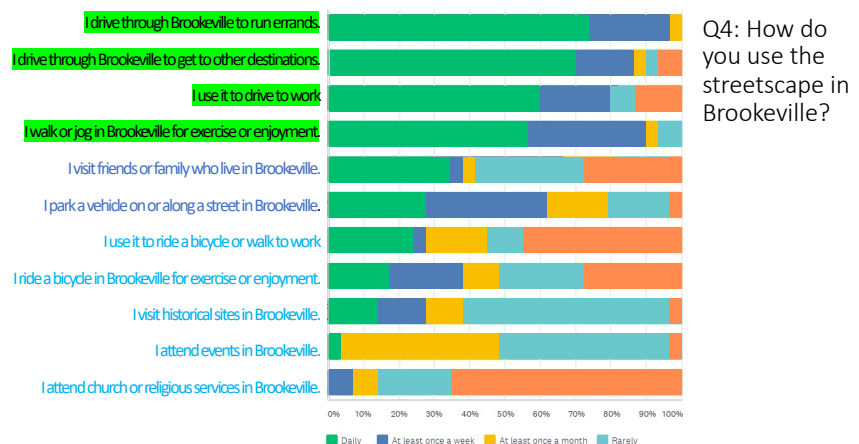


Figure 10 - Most used words to describe what respondents liked best about Brookeville and its most distinguishing characteristics



Figure 10 - Features respondents would like to see changed

Transportation, Pedestrian and Streetscaping

Based upon the input provided during the initial Town tour, by those that attended the two public meetings (one virtual and one in person), and those that responded to the questionnaire, the overall vision and goals for the Comprehensive Plan should be updated to reflect the following transportation related goals:

- **Recapture** High and Market as local streets.
- **Reconnect** a network of sidewalks and trails.
- **Restore** Brookeville's public streetscape and its historic sense of place.

Recapture High and Market as Local Streets

To recapture High and Market Streets as local streets, traffic calming measures will be needed to give stronger visual clues to drivers that they are entering a settled place and they need to slow down. Traffic calming measures appropriate for a Historic District listed on the NRHP need to rely upon the context of the street to give drivers the necessary visual clues, rather than inserting distinctive measures that shift the horizontal or vertical alignment of the roadway.

To achieve the stated goal of recapturing High and Market Streets as local streets while retaining the character defining features of this historic community, traffic calming measures need to be geographically structured as drivers come into town into three distinct character areas:

Gateway (large pink circles)

- Provides a distinctive entrance to town clearly stating that drivers are entering a settled place
- The road character changes from a highway (MD 97) to a street (High Street)

Transition (yellow)

- Provides for the gradual narrowing of the look and feel of the street

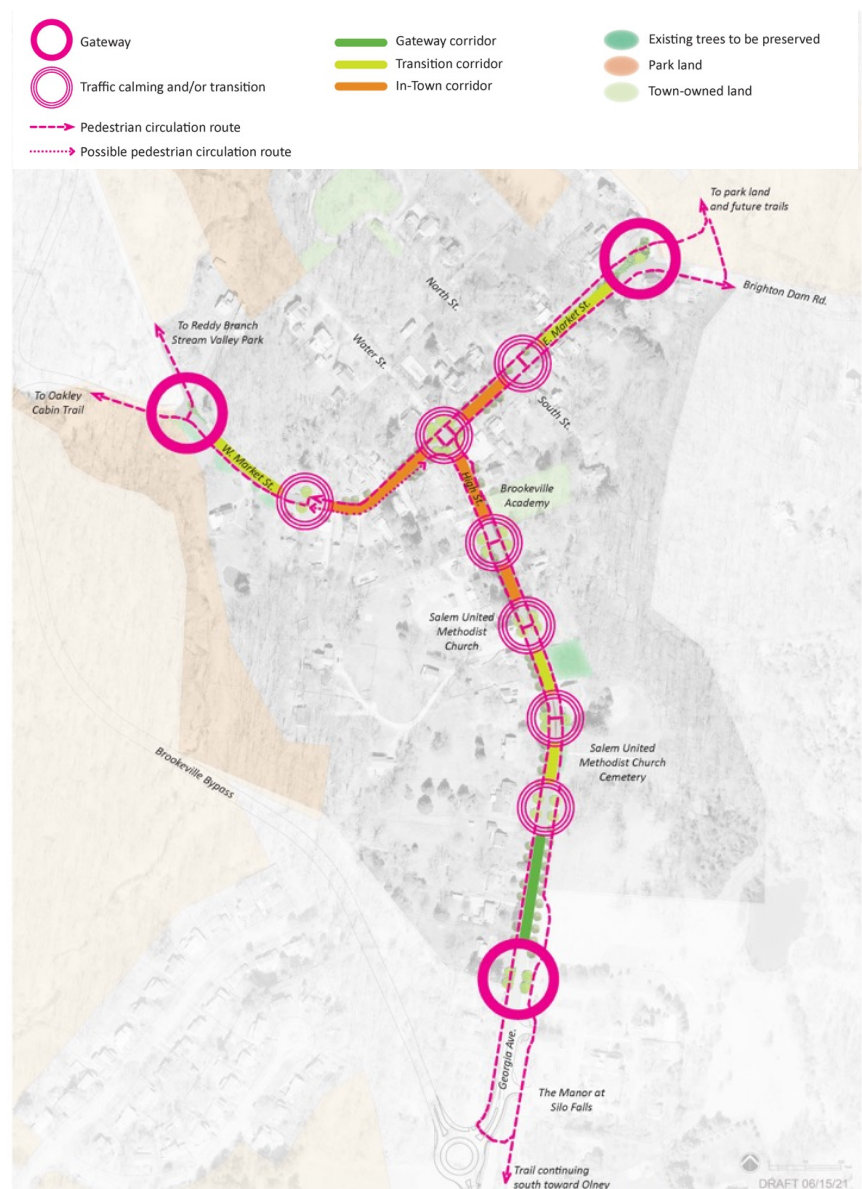


Figure 11 - Overall traffic calming concept

- On Georgia Avenue at the south end of town, where the contributing historic features are secondary and from the mid-20th century, one horizontal shift in alignment can be accommodated and engineered at the desired speed limit (preferably 25 mph).

In Town

- Within the core area of the Town (orange), sidewalks are wider and located on both sides of the street with on-street parking. Streets are part of the public spaces and pedestrian are the primary users, with vehicles secondary.
- Placemaking

Systematic Safety

An organizational construct for increasing vehicular and pedestrian safety in Brookeville are five underlying principles of the Vision Zero movement referred to as “Systematic Safety.”¹ Each of the five principles is applied differently depending upon the context:

1. Speed control and separation - either provide more separation at higher speeds or use self-enforcing speed reduction measures
2. Functional harmony - avoid incompatible functions
3. Predictability and Simplicity - visual clarity of each function is readily apparent
4. Forgiveness and Restrictiveness - people make mistakes and they should be anticipated
5. State awareness - what people do when they walk, ride or drive (e.g., distractions)

The systematic safety principles can be utilized to organize the measures needed to transform Georgia Avenue/High Street/W. Market Street from a minor arterial to a local street. The use of self-enforcing speed reduction measures is critical to the overall goals. Based upon these principles the following operational measures should be considered as traffic calming and pedestrian safety measures are implemented over time:

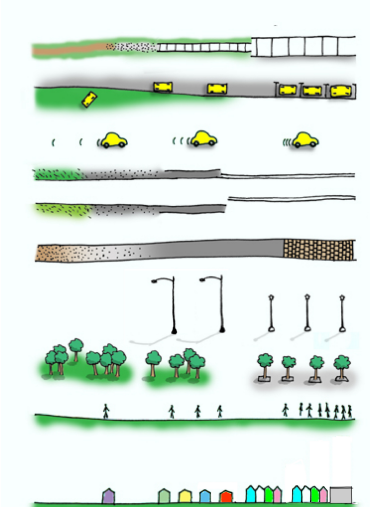
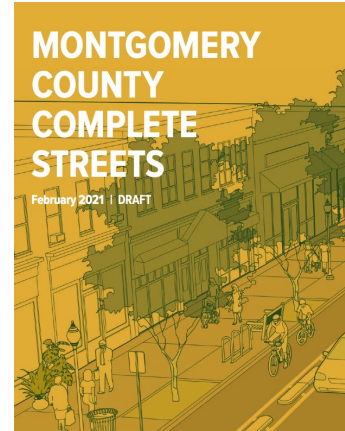
	Rural	Transition	Village	Element	Gateway	Transition	In-town
				Curb/ Shoulder	Open Flush	Curb and Gutter Pan (concrete)	Header Curb (granite)
				Parking	Off-street	Off Street	On-street
				Sidewalks/ Pathways	Separated asphalt shared use path	Sidewalk with continuous planting strip	Wide sidewalk with tree pits
				Lane Width	12'	11'	10'
				Lighting	Overhead at intersections	Ornamental roadway	Historic roadway and sidewalk
				Street Trees	Wider Spacing, full broad crown	Decreasing spacing, species vary at measures	Close spacing, enclosed canopy
				Placemaking	Visual Features within gateway	Tactile features along sidewalks	Focal features in public spaces

Figure 12 Self-reinforcing details organized to provide predictability and simplicity along with visual clarity for each functional element

¹ See Peter J. Furth, *Systematic Safety: The Principles Behind Vision Zero*:
<https://www.youtube.com/watch?v=5aNsWvNYKE&feature=youtu.be&t=72>

- Reduce the posted speed limit to 20-25mph (20mph is preferable from a pedestrian safety perspective) in Town and reinforce the speed limit reductions with a splitter island on Georgia Avenue south of the cemetery coupled with narrowing the travel lanes from 12' to 11'(transition) to 10' or 11' (in town).
- Reduce shoulder widths approaching town to provide more room for sidewalks separated from traffic. In transition areas and in town, bicycles can coexist with motorized traffic when speeds are below 25mph, and traffic volumes are very low. A waiver from State Bicycle Policy may be needed if the route continues as a state highway.
- Structure details related to the roadway cross section to provide clear and predictable patterns associated with the transition from state highway to local street (as shown in Figure 12).
- Provide clear and visible crosswalks at desire lines and incorporate those crosswalks into traffic calming measures.



Montgomery County Vision Zero and Complete Streets Policies and Planning

The recommendations contained in this plan amendment are consistent with the draft Montgomery County Complete Streets Design Guide currently under review by Montgomery County Planning Board and Council. Although there are no specific street types for “rural historic village,” the neighborhood street most closely matches the standards that would be appropriate for Brookeville, post bypass.

According to the revised Planning Board Draft of Chapter 2 dated February 2021:

“Neighborhood Streets serve predominantly residential areas with low volumes of motor vehicle traffic. Pedestrian and bicycle activity are common along these streets. Neighborhood streets should have sidewalks on both sides of the street, though in retrofit applications, a sidewalk on one side of the street may be an interim objective in existing neighborhoods with

G. Neighborhood Streets

Neighborhood Streets serve predominantly residential areas with low volumes of motor vehicle traffic. Pedestrian and bicycle activity are common along these streets. Neighborhood streets should have sidewalks on both sides of the street, though in retrofit applications, a sidewalk on one side of the street may be an interim objective in existing neighborhoods with limited right-of-way. Most, but not all, Neighborhood Streets in Montgomery County offer on-street parking. Design for Neighborhood Streets should focus on encouraging slow speeds, pedestrian safety, healthy street trees, and well-defined routes to nearby parks, transit, and schools.

- MB** Maintenance Buffer
- SW** Sidewalk
- SB** Street Buffer
- P** Parking Lane
- TV** Travel Lane

Key Features:

- » **Development intensity:** Moderate- to low-intensity residential development
- » **Pedestrian and bicycle activity:** Moderate
- » **Vehicle activity:** Low volumes of personal vehicles
- » **Transit service:** Typically, limited or none
- » **On-street parking:** Varies/ context-dependent
- » **Other key features:** Frequent curb cuts/driveways, street trees



Figure 13 - Excerpt from Montgomery County Complete Streets Design Guide for Neighborhood Streets

Figure 2-55. Neighborhood Street

limited right-of-way. Most, but not all, Neighborhood Streets in Montgomery County offer on-street parking. Design for Neighborhood Streets should focus on encouraging slow speeds, pedestrian safety, healthy street trees, and well-defined routes to nearby parks, transit, and schools.”

MDOT has developed a similar approach for state roads entitled “Context Driven: Access and Mobility for All Users.” The recommendations in this plan amendment are also consistent with those in the MDOT approach². However, the MDOT approach does not appear to address state controlled local streets.

Should the Town of Brookeville, Maryland DOT and Montgomery County come to a future agreement about maintenance responsibilities and modifying the classification from Minor Arterial to Local Street for Market and High Streets, then the Town should work with Montgomery County to use the Complete Street Design Guide to implement the plan amendment recommendations.

Table 2 - Comparison of Design Guidance for MDOT and MOCO Complete Streets Draft (February 2020)

	MDOT	MOCO Complete Streets DRAFT
TARGET SPEED	<40mph	20-35mph
LANE WIDTH: Secondary Highways with a posted speed of 40 mph or less	11’ Minimum	11’ Minimum
LANE WIDTH Neighborhood Street	NA	10.5’ Minimum
Curb and Gutter	Type A in kind only Type A combination	
Intersection Turning Radii (minor road)	30’ minimum	15’ (typical)
Sidewalk	5’	6’

Assumptions

The following assumptions are guiding the recommendations for the plan amendment:

- Traffic on High Street will be reduced from 10,400 to 900 (ADT)
- High Street and Market Street reclassified as a local street
- Request reduction in posted speed limit to 25 mph
- Montgomery Complete Streets Draft preferred guidance

² <https://experience.arcgis.com/experience/3476e680584c49e48303fe6d52ceeda9>

Traffic Calming Elements

Figures 14, 15 and 16 provide examples of the types of traffic calming measures that can be inserted into the gateway and transition areas approaching Brookeville. The splitter island is only appropriate in the area south of town identified with secondary historic resources from the mid-20th century.

Along Georgia Avenue the detail transition from the asphalt path to brick sidewalks should begin at the proposed splitter island. The sidewalk to be replaced by as part of Parcel 770 should be located and have a similar character as Figure 16 with a wider planting street and street trees creating the transition toward the in-town details that start at the proposed splitter island.

Lighting along Georgia Avenue should follow a similar transition with roadway lighting and higher illumination levels at the roundabout transitioning to lower historic streetlamps and standards starting at the splitter island. Illumination levels should gradually change from the higher speeds at the edge of town to the lower speeds in town to avoid high contrasts in lighting levels. Lower illumination levels (but closer together) in town are appropriate to slower desired operating speeds. The higher illumination levels for the standard cobra light fixtures should be removed and replaced with lower pole mounted light standards appropriate to an historic area as part of the utility undergrounding effort.

Within the Core Area, the primary traffic calming measure is to make the Town look and function more like a village with wider sidewalks on both sides of the streets, narrower travel lanes, street trees, on-street parking on one or both sides of the street, as can fit dimensionally, and undergrounding overhead utility lines.

East Market Street Intersection Controls

Additional pedestrian and vehicular safety measures are recommended for East Market Street to address oncoming westbound traffic from Brighton Dam Road. The speed humps should be removed and replaced with four way stop control at the North/South/East Market Street intersection. In addition, four way stop control should be installed at the intersection of East and West Market with Water and High Streets and the intersection and Water Street realigned with High Street as much as practical. The Market/Water/High Street intersection should be reconstructed as a raised intersection and paved with vehicular clay pavers (Figure 21).

Traffic Calming Elements: Gateways and Transition Areas



Figure 14 - Flush curbs in gateway areas clue drivers that they are entering a settled place (Upperville, VA)



Figure 15 - At the southern end of town utilize a narrow splitter island as a traffic calming measure coordinated with the location of a proposed crosswalk (Upperville, VA)



Figure 16 - Use street trees in both gateway and transition areas to enclose driver views and increase "visual friction" (Source: <https://howwedrive.com/2008/08/17/natural-traffic-calming/>)

Reconnect a Network of Sidewalks and Trails

The proposed goal of establishing a continuous network of sidewalks and looped walking paths is comprised of three distinct objectives:

- Establish a network of sidewalks along High and Market Streets
- Expand the network of natural surface trails to connecting parkland that surrounds Brookeville
- Increase the convenience and safety of non-motorized access to Olney

Establish a network of sidewalks along High and Market Streets

Existing sidewalks, as shown in Figure 6, page 7, should be extended along both sides of the street to the south along High Street/Georgia Avenue to the new roundabout and west along Market Street towards Reddy Branch Stream Valley Park.

Along Georgia Avenue, sidewalks can be constructed along both sides of the street if the roadway is narrowed to include only the travel lanes and curb and gutter (24' width total). Sidewalks can then be installed on both sides utilizing the recaptured space from the existing shoulders. This will require a waiver from MDOT statewide bicycle policy. The rationale for the waiver is that there is not enough space for bicycle and pedestrian accommodations without expanding the right-of-way. The town, through the various public outreach methods utilized to develop this plan amendment, has clearly stated a preference for sidewalks on both sides of the street. Bicycles can be accommodated by sharing the existing travel lanes once the Bypass is opened and the traffic is reduced to significantly low volumes consistent with a local neighborhood street.

Two locations of immediate concern are to establish a sidewalk on the west side of the wall at Salem United Methodist Church Cemetery to eliminate the impact of walkers on existing gravesites closest to the road and

The “active zone” between the curb and property line



Figure 17 - An asphalt shared use pathway with street trees is recommended in the gateway areas approaching town (courtesy of Ohio River Valley Trail, Louisville, KY)



Figure 18 - Brick sidewalks with planting strips are recommended for transition areas and core areas within the Town



Figure 19 - Within the core area of town, the parking areas should be paved with permeable pavers (granite cobbles or textured concrete pavers with similar character to cobbles with engineered subbase designed to encourage infiltration for stormwater)

to coordinate the sidewalk location with the development of Parcel 770, just south of the Cemetery. The existing asphalt path from Silo Falls should curve toward the street and the sidewalk should be developed at the street, similar in character as shown in Figure 17.

Along West Market, room for the sidewalk can be found by making slight adjustments to the alignment of the roadway and building out a curb on the north side of the road to accommodate a four foot sidewalk; then crossing over at the high point of the curve with greatest visibility (see location "P" on Figure 24, page 18). If the roadway is ever reconstructed, sidewalks can be built on both sides between the crosswalk and town, if sensitive historic and adjoining properties are not impacted during construction.

Crosswalks

Pedestrian use of the network will increase significantly with appropriately located crosswalks. The following crosswalk locations are recommended with letters referencing the locations shown on Figure 24, page 18).

- Salem United Methodist Church Cemetery (E) as part of the recommended splitter island, which also serves as a pedestrian refuge
- Salem United Methodist Church parking entrance (F)
- Salem United Methodist Church to Brookeville Academy (H)
- Market Street at South/North Streets intersection (L) – note speed humps should be removed on Market Street
- West Market mid-block sidewalk crossing (P)
- West Market/Brookeville Road Gateway pedestrian crossing to connect to Reddy Branch Stream Valley Park trail system (Q)

Crosswalks



Figure 20 - High visibility "Continental" type crosswalks can be striped to reduce tire track wear through the gaps and are recommended for gateway and transition areas to increase pedestrian safety



Figure 21 - Within historically sensitive areas associated with the Primary Historic Resources, brick crosswalks can be installed and striped using a concrete band (need to contain the bricks) tinted white to increase visibility.



Figure 22 - A raised intersection with brick pavers suitable for roadway use will help to define the core of the Town as a pedestrian space rather than one dominated by the roadway

Expand the network of natural surface trails to connecting parkland that surrounds Brookeville

This Plan amendment continues the 2010 Comprehensive Plan's encouragement of the expansion of pedestrian access to both existing sidewalks as well as the parkland that borders the Town. Establishment of pedestrian paths to access the existing public open space along Reddy Branch and the Thomas Mill Race is encouraged.

With the completion of the Bypass, the Town will be surrounded by parkland on three sides. A network of natural surface trails around the Town should be completed in cooperation with M-NCPPC Montgomery County Parks. Limited existing public access to the surrounding parkland should be expanded by connecting the Town's sidewalks along East Market Street with the MNCPPC property surrounding the Washington Suburban Sanitation Commission's pumping station. A trail between the pumping station and the portion of Georgia Avenue to be abandoned on the south side of Reddy Branch could then connect with the trail to the Oakley Cabin Trail. With the completion of the Bypass north of town, a short portion of Georgia Avenue between Brookeville Road and the northern terminus of the Bypass (including the existing bridge over Reddy Branch) would provide access to the northern side of Reddy Branch within M-NCPPC parkland. A trails plan is needed for the Reddy Branch Stream Valley Park to advance these connections in a timely manner (M-NCPPC responsibility).

An example of the expansion of access to natural areas in Town can be found in the 2007 construction of a small pedestrian walkway covered in wood chips as part of the development of a two-house subdivision—Powers' Wood, and adjoining and integrated with a small natural park area north of the restored Brookeville Schoolhouse. To take advantage of the parklands that border the Town, the Plan encourages the development of other pathways in natural settings.

Extending the Network

Beyond creating pathways to make the adjoining stream valley parks and natural areas more accessible, the plan emphasizes the importance of linking the Town to existing pedestrian and public transportation infrastructure. The Town would like to encourage pedestrian and bicycle transportation links to nearby areas such as Olney by creating a safe and accessible network of demarcated crossings and paths. With the anticipated reduction in traffic volume and implementation of traffic calming measures to reduce operating speeds, the Plan recommends additional measures to increase access to all modes of transportation, utilizing the fundamental concepts of complete streets as illustrated in Montgomery County's draft Complete Streets Design Guideline.



Figure 23 Conceptual diagram of natural surface trail utilizing existing parkland and public right-of-way (long dashed line). Trail easements would be required along the stream valley from Manor at Silo Falls to Brighton Dam Road (short, dashed line)



Key

- (A) Gateway with grouping of ornamental trees
- (B) Trees flanking the roadway
- (C) Grouping of trees at transition
- (D) Sidewalk located west of the cemetery wall
- (E) Splitter island with pedestrian crossing and town sign
- (F) Pedestrian crossing and curb extensions, south of Salem UMC
- (G) Vegated swales and street trees on both sides of roadway
- (H) Pedestrian crossing and curb extensions, north of Salem UMC
- (I) On-street parking area with permeable pavers
- (J) Raised intersection with 4-way stop with curb extensions and crosswalks

- (K) On-street parking with permeable pavers
- (L) Crosswalk and curb extensions
- (M) E. Market St. Gateway; frame Bentley House with trees; install ornamental shrubs on the hillside; restore stone wall at town sign
- (N) Understory trees suitable for overhead lines
- (O) Long-term sidewalk option
- (P) Crossing location with advance warning signs
- (Q) W. Market St. Gateway with pedestrian crossing; install ornamental shrubs and locate entrance sign on north side of the roadway
- Existing roadway trees to be preserved
- Park land
- Town-owned land

Figure 24 - Recommended Concept Plan for Transportation, Pedestrian Networks & Streetscaping Elements

Restore Brookeville's Public Streetscape and its Historic Sense of Place

Critical to the success of the recommendations included within this plan amendment is the full integration of complete street concepts—directing those that design and make changes to Brookeville's streets to routinely design and operate the entire right of way to prioritize safer slower speeds for all people who use the road, over high speeds for motor vehicles—all utilizing context sensitive design and materials inserted carefully into the historic fabric of Brookeville.

To that end, the following examples were developed to illustrate the concepts and recommendations included with this plan amendment. The illustrations are meant to be explanatory and illustrative of the concepts and to not reflect a specific design or engineering solution. Brookeville's right-of-way is truly historic, in that while GIS level data can communicate approximate locations of the right-of-way, the actual locations and existing conditions need to be surveyed and public right-of-way confirmed.

In addition, more detailed design and engineering is needed to translate the concept so it works with the more detailed survey and design level knowledge of existing conditions, changes to adjacent uses and requirements for stormwater management, environmental compliance, engineering, and the historic area. Use of any federal funding sources will require compliance with federal environmental requirements such as NEPA.

The conceptual illustrations are organized by major street and in the direction approaching the core of the Town.



Figure 25 - Location reference for conceptual illustrations that follow



- Street trees/visual enclosure
- Sidewalk
- Vegetated swale
- Existing asphalt path

Figure 26 - Georgia Avenue/High Street Gateway illustrating separated shared use path and street trees to begin narrowing the look and feel of the roadway



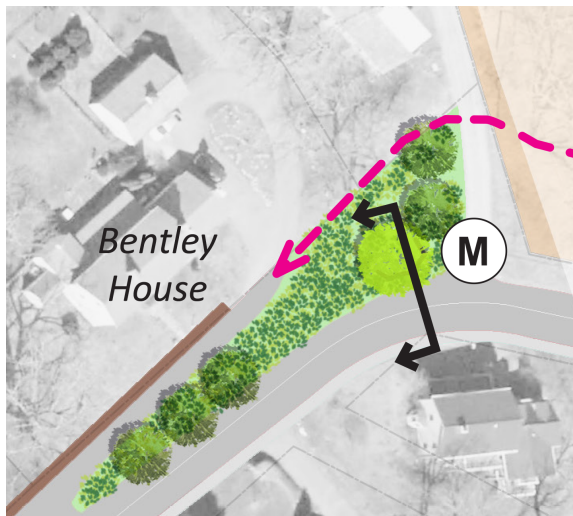
- Street trees/visual enclosure
- Brick sidewalk
- Sidewalk buffer

Figure 27 - High Street Transition illustrating the use of a splitter island as a traffic calming measure as well as a refuge island to protect pedestrians in the proposed crosswalk



- Vegetated buffer/street trees
- Brick sidewalk
- Vegetated buffer

Figure 28 - High Street showing how street trees with a vegetated buffer to separate the sidewalk on both sides using space captured by reduced shoulder widths. Where parking is desired, the buffer area can be adapted as curb extensions to accommodate parking.



- Sidewalk
- Gateway sign, restored wall and plantings

Figure 29 - East Market Street gateway located at the highly visible curve from Brighton Dam Road incorporating a restored stone wall and flowering trees and shrubs with the gateway sign

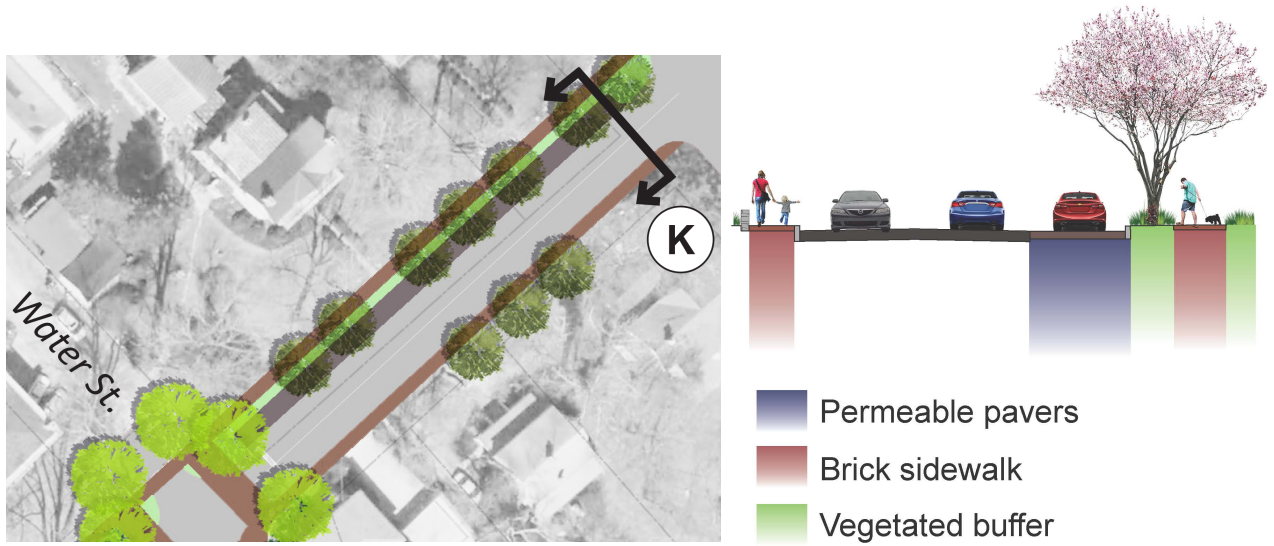


Figure 30 - Market Street in town showing how the on-street parking areas also function for stormwater management by utilizing engineered soil below the pavement

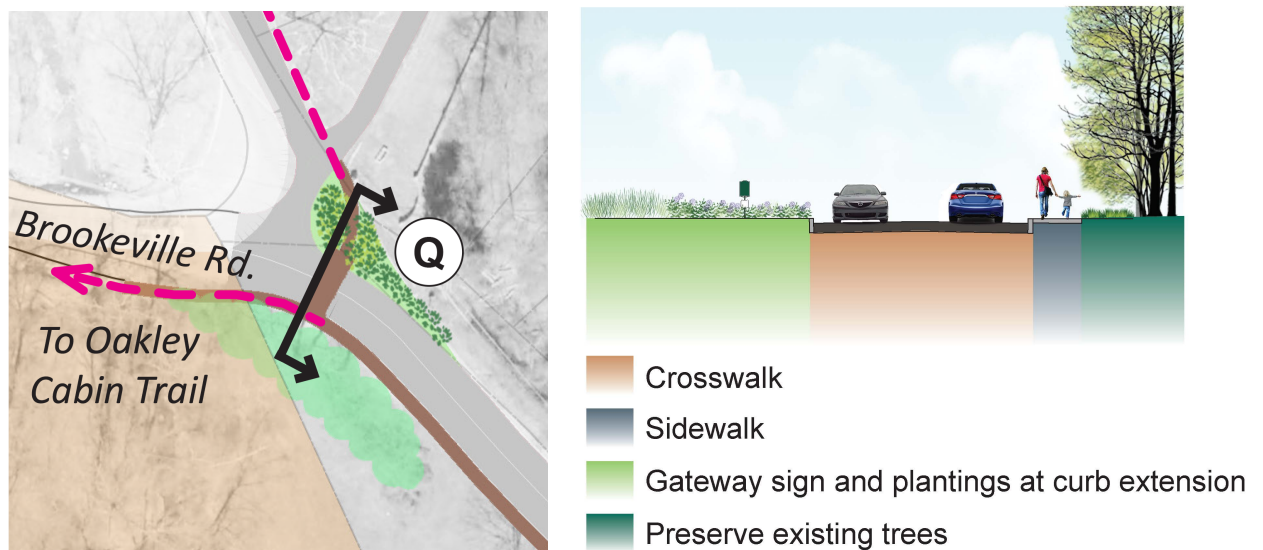
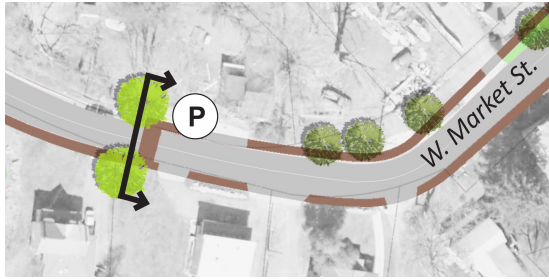
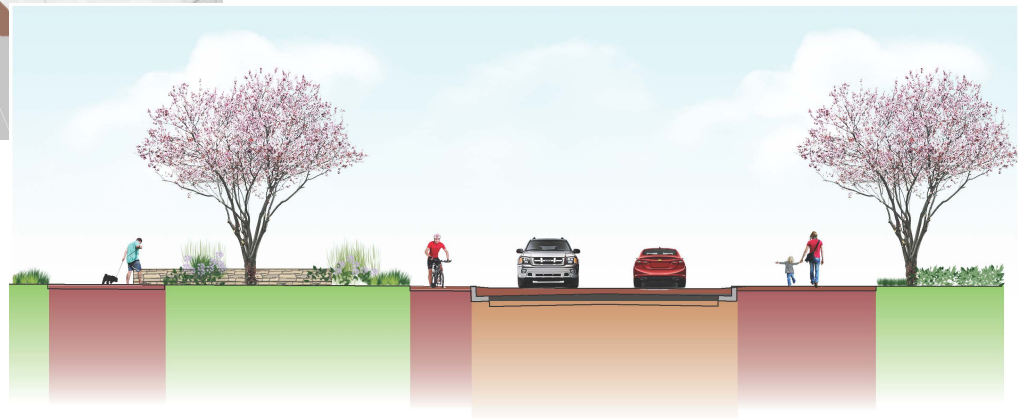


Figure 31 - West Market Gateway with crosswalk and sidewalk extension leading to the Oakley Cabin Trail to the south and Reddy Branch Stream Valley Park to the north



- Street trees/visual enclosure
- Brick sidewalk
- Crosswalk

Figure 32 - Proposed crosswalk at the location with the best sight lines for oncoming traffic. The sidewalk shown on the south side of Market east of the crosswalk is a long-term option and would only be built if the roadway is reconstructed and any potential impacts of construction to adjoining historic properties can be addressed.



- Street trees/visual enclosure
- Brick sidewalk
- Raised intersection/crosswalk

Figure 33 - A raised intersection is recommended at the High/Market/Water Streets intersection utilizing a different type of tree silhouette that would work under the wires as well as call more attention to the intersection.

Additional Design Issues and Details

Implementing the concepts and recommendations will require careful attention to context sensitive design and selection of materials. In addition, maintenance considerations may factor into design decisions, especially where and when to use brick sidewalks. The following are general recommendations.

Curb and Sidewalk

Within the core areas of town, consideration should be given to utilizing granite curbs, brick sidewalks, brick crosswalks held in place with concrete header curbs, and using the historically appropriate light fixtures.

Within the roadway, larger vehicular clay pavers (bricks) should be utilized that meet ASTM 1272 for heavy vehicular traffic. Brick color can be selected depending upon source, but traditional brick coloration is recommended. Where crosswalks

meet the curb, ADA requirements stipulate a contrasting material be utilized as a detectable warning—"a standardized surface feature built in or applied to walking surfaces or other elements to warn visually impaired people of hazards on a circulation path." In historic districts it may be possible to utilize a contrasting color to the sidewalk and crosswalks with truncated domes that is not bright yellow.

Construction of brick sidewalks, if done appropriately, require the same kinds of maintenance as concrete sidewalks. Brick sidewalks should be placed in a sand setting bed on top of a flexible pavement, such as asphalt. Geotextile fabric can also be utilized to limit weed growth and to keep the subsurface materials in place. A well graded and fully compacted to industry standards sub-base should support the flexible pavement and sand setting bed so that it is as strong as a concrete sidewalk. The sand setting bed and flexible pavement system offers room for the typical freeze-thaw cycles found in Brookeville. Snow removal on brick sidewalks, if carefully constructed, can be accomplished with hand shovels, snow blowers (rubber blade tips), and sand (rather than de-icing salts). If de-icing salts are needed, then they can be mixed with sand and applied in a limited way after as much snow and ice has been removed and then washed or swept off the brick surface after melting and drying.

Granite curbs have the following advantages over standard concrete for curb and gutter within historic districts:



Figure 34 - Use of vehicular clay pavers (crosswalks), granite curbs, traditional brick pavers (sidewalk) and historically appropriate light fixtures (Middleburg, VA)

- Material is attractive with high visual integrity in and of itself, therefore the use of granite would have no adverse effect on historic district's significant historic resources
- Repairs are minimal and can simply be reused and reset if damaged, therefore eliminating issue of visual contract when curb is repaired or replaced
- Granite withstands much of the damage from vehicles requiring a minimum repair (reusing and resetting the curb, primarily)

The curbs are installed as header curbs without gutter pan (the pavement goes right up to the curb base). A premium cost on an order of magnitude basis is likely to be 25% higher than concrete (although many factors such as distance to quarry and availability of skilled labor could impact cost). This is typically offset over time by maintenance and replacement of concrete or rolled asphalt curbs which are prone to damage from vehicles and environmental conditions (spalling).

Existing extruded asphalt curbs should be replaced with concrete curb and gutter in gateway areas and granite curbs within transition and in town.

Light Fixtures

The light fixtures should be reorganized in town with the most historically sensitive fixture relocated for the core area and transition area. In gateway areas, more standard street lighting may be needed to provide the necessary illumination with the higher vehicular operating speeds.

As more walkways are constructed, lighting levels should be gradually increased approaching the core area of town and at all crosswalks.



Figure 35 - Existing extruded asphalt curbs should be replaced with concrete curb and gutter in the gateway area (and with granite curbs in transition areas and in town). A flush curb should be installed in advance of the gateway to alert drivers that they are entering a settled place.



Figure 36 - Slower vehicular operating speeds will reduce or eliminate the need for area roadway lighting in town or transition areas. Gateway areas may require area roadway lighting.



Figure 37 - Existing pole mounted light fixture used throughout the historic district. As new sidewalks are constructed, more of these light fixtures should be added to gradually increase street and walkway lighting within town to increase night visibility for pedestrians.

Stormwater Management

Almost all paving or hardscape improvements to the streetscape will require compliance with state and local stormwater management regulations to determine how increased impervious surfaces will impact the macro and micro watersheds within the Town. The impacts of stormwater can be reduced using permeable paving, as shown in Figure 19 on page 15 and Figure 38, and vegetated buffer strips, as shown in Figure 39 and Figure 40.

It is recommended that on-street parking areas be constructed with permeable pavers. Paver color should complement the red brick sidewalks and granite curbs.

Stormwater Management



Figure 38 - Permeable pavers allow stormwater to be filtered, stored, and or redirected to a stormwater management facility



Figure 39 - Vegetated buffer strips can store and filter stormwater



Figure 40 - Plantings within vegetated buffers can be as simple as turf grass areas or herbaceous perennial groundcovers. Seating can be incorporated within these spaces

Tree Selection Appropriate for Under Overhead Electric Lines

If overhead utility lines cannot be buried, it is still feasible to plant small trees that generally do not exceed 20' in height. Such trees include Eastern Redbud (*Cercis canadensis*), Cornelian Cherry (*Cornus mas* 'Spring Glow'), White Fringetree (*Chionanthus virginicus*), and Serviceberry (*Amelanchier canadensis*). These examples are regionally native, flowering trees.



Figure 41 - Small street trees that generally do not grow taller than 20', like this Forest Pansy Redbud, are appropriate for planting under overhead utility lines

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Town of Brookeville Comprehensive Plan

Update to Transportation, Pedestrian Networks & Streetscaping Elements

Brookeville, Maryland

ATTACHMENT ONE: Questionnaire Results



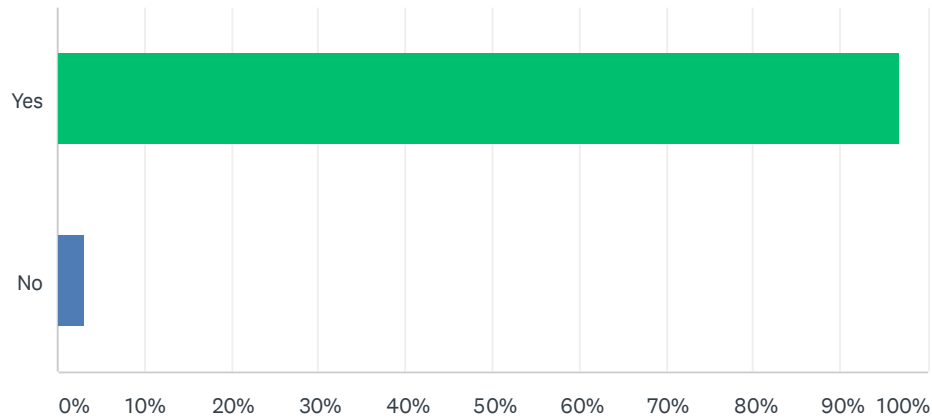
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LANDSCAPE ARCHITECTS, PC

July 23, 2021

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Q1 Do you live in Brookeville?

Answered: 32 Skipped: 0



ANSWER CHOICES	RESPONSES	
Yes	96.88%	31
No	3.13%	1
TOTAL		32

Q2 In just a few words or a phrase, what do you like best about Brookeville?

Answered: 31 Skipped: 1

#	RESPONSES	DATE
1	The surrounding environment/nature	5/11/2021 9:58 PM
2	I like the greenscape/landscape and the look of the homes	5/11/2021 8:17 PM
3	How everyone is friendly and neighborly.	5/10/2021 10:27 AM
4	the small town nature and neighborly feel	5/5/2021 8:22 PM
5	I like Brookeville as a small community that is relatively isolated by geography and infrastructure yet close to everything at the same time. Bracket by woods, we are raising our kids in an environment very similar to the way I grew up in the 60s and 70s. That is a hard thing to find in this century.	5/5/2021 3:00 PM
6	Beautiful historical town	5/5/2021 7:50 AM
7	Small, historic and picturesque	5/4/2021 8:06 PM
8	historic flavor	5/4/2021 7:57 PM
9	good neighbors, history, old houses, trees	5/4/2021 5:51 PM
10	Distinct community	5/4/2021 5:22 PM
11	I love that it is incredibly unique and lush but tucked away so it's a hidden secret (yet still so close to amenities!)	5/4/2021 3:52 PM
12	The historic aspect, the natural surroundings, the people	5/4/2021 3:47 PM
13	Quiet	5/4/2021 3:10 PM
14	I love the small town feel and friendliness of the people.	5/4/2021 2:38 PM
15	The nature, history and quietness of the town	5/4/2021 12:32 PM
16	It is truly a town where we know all of our neighbors	5/4/2021 11:31 AM
17	The residents all care about each other and the town is a peaceful, relaxing place to live.	5/4/2021 11:22 AM
18	its charming, picturesque streetscapes, its well preserved historic character	5/4/2021 10:52 AM
19	History	5/4/2021 10:40 AM
20	So many people who care so much about the Town.	4/27/2021 5:59 PM
21	sense of community, unique and quirky town, preserving history	4/27/2021 11:45 AM
22	The people of Brookeville. The props National history of Brookeville. The homes and the very natural surroundings.	4/25/2021 2:24 PM
23	The character of the homes and the people.	4/24/2021 9:22 PM
24	Nice quaint central location (and eventually quiet).	4/24/2021 4:43 PM
25	Historic Character, Walkability, Safe Place to Live	4/24/2021 12:35 PM
26	Beautiful town with unique homes and properties.	4/24/2021 10:43 AM
27	It is a nice community to live in	4/23/2021 4:15 PM
28	Quiet, secluded, private, calm	4/22/2021 11:49 PM
29	Charming historic town with potential	4/22/2021 6:17 PM

30	The quiet neighborhood feel with little to no outside people and traffic deal with.	4/22/2021 6:01 PM
31	it's quiet	4/22/2021 4:44 PM

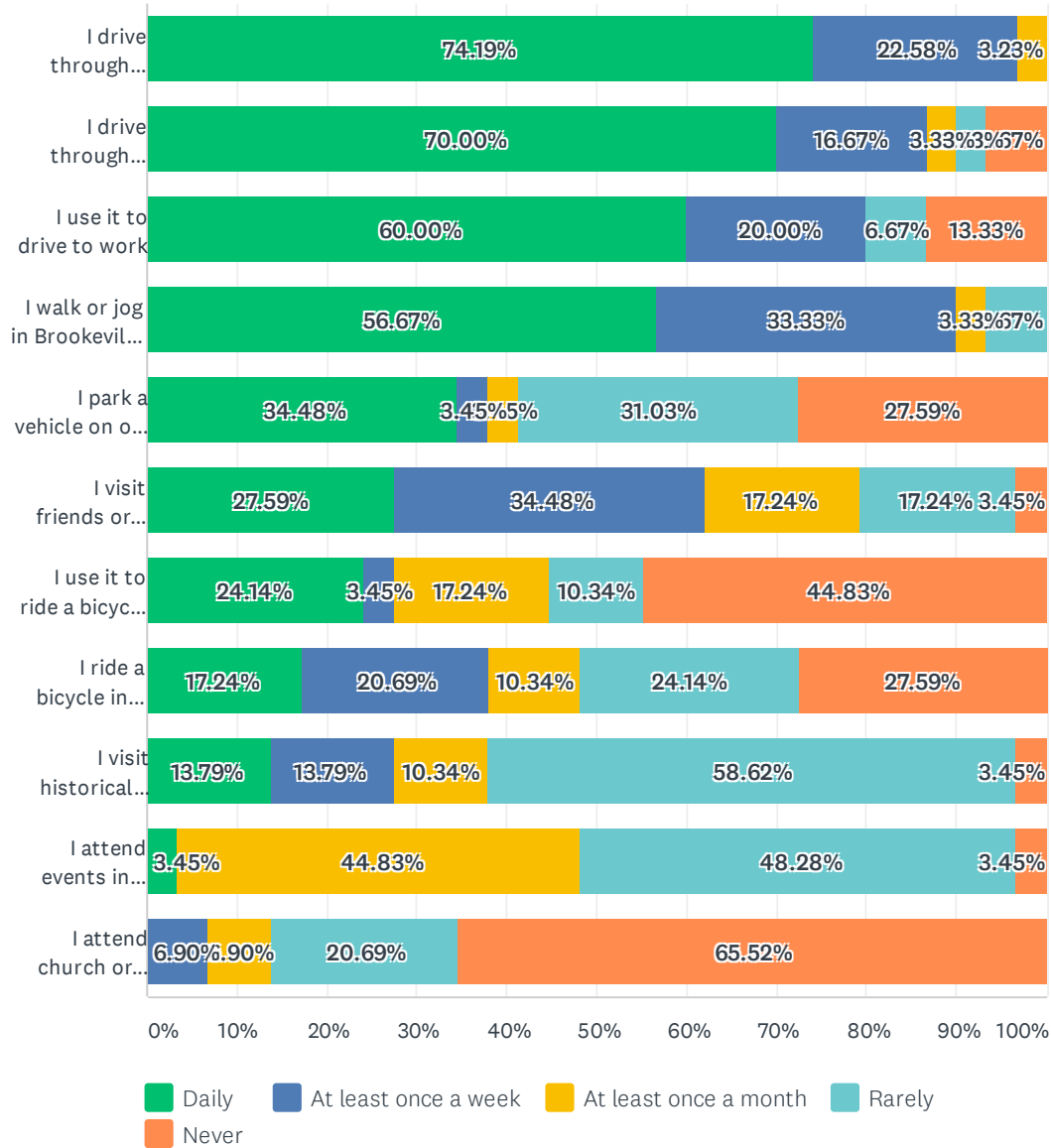
Q3 In just a few words or a phrase describe the most distinguishing characteristic of Brookeville's streetscape (built and natural elements of the designed street corridor)?

Answered: 26 Skipped: 6

#	RESPONSES	DATE
1	The Academy, streetlights	5/11/2021 9:58 PM
2	We love the look of the town with regards to design of the streets and homes.	5/10/2021 10:27 AM
3	historic homes	5/5/2021 8:22 PM
4	We are surrounded by woods and streams on three sides. I love the charm and character of the brickwork side walks. The roadways themselves are wholly unremarkable. What old fashioned lamp posts survive, are a nice anachronistic element.	5/5/2021 3:00 PM
5	Dramatic and challenging topography, eclectic styles and varied dates of construction	5/4/2021 8:06 PM
6	Current streetscape? Not much.	5/4/2021 7:57 PM
7	Inaccessible one side of Town to the other, difficult to traverse as many uneven surfaces and no sidewalks/safe connection to the walking path along Georgia Ave.	5/4/2021 5:22 PM
8	Brick patterned sidewalks, houses built right close to the roads, historic-looking homes and communal buildings.	5/4/2021 3:52 PM
9	They need some work, but have a lot of promise.	5/4/2021 3:47 PM
10	Historic looking - lots of variety	5/4/2021 3:10 PM
11	It's almost walkable but actually quite dangerous.	5/4/2021 2:38 PM
12	The historic homes, front porches and tree lined streets.	5/4/2021 11:22 AM
13	lined with charming buildings, lots of gardens, brick sidewalks	5/4/2021 10:52 AM
14	market street	5/4/2021 10:40 AM
15	The Brookeville Academy	4/27/2021 5:59 PM
16	rustic	4/27/2021 11:45 AM
17	Rustic look of the gravel roads.	4/25/2021 2:24 PM
18	Beautiful mature nature and architecture.	4/24/2021 9:22 PM
19	Tons of potential, after the bypass goes in.	4/24/2021 4:43 PM
20	Historic Character and small town feel/look. Academy, Madison House, and Valley house are anchors that hold down our historic character	4/24/2021 12:35 PM
21	Scenic, peaceful appearance.	4/24/2021 10:43 AM
22	There really is not a distinguishing characteristic of Brookeville. Maybe the Brookeville Academy	4/23/2021 4:15 PM
23	historic, unique, every home is different, every property has its own character	4/22/2021 11:49 PM
24	dangerous, with some nice homes and some delapidated	4/22/2021 6:17 PM
25	The brick sidewalks are great. The gravel roads are a nightmare to drive on, an eye sore to look at and financial drain all year round and need an upgrade	4/22/2021 6:01 PM
26	simplicity	4/22/2021 4:44 PM

Q4 How do you use the streetscape in Brookeville?

Answered: 31 Skipped: 1



Brookeville Questionnaire: Transportation, Pedestrian Networks & Streetscaping Elements

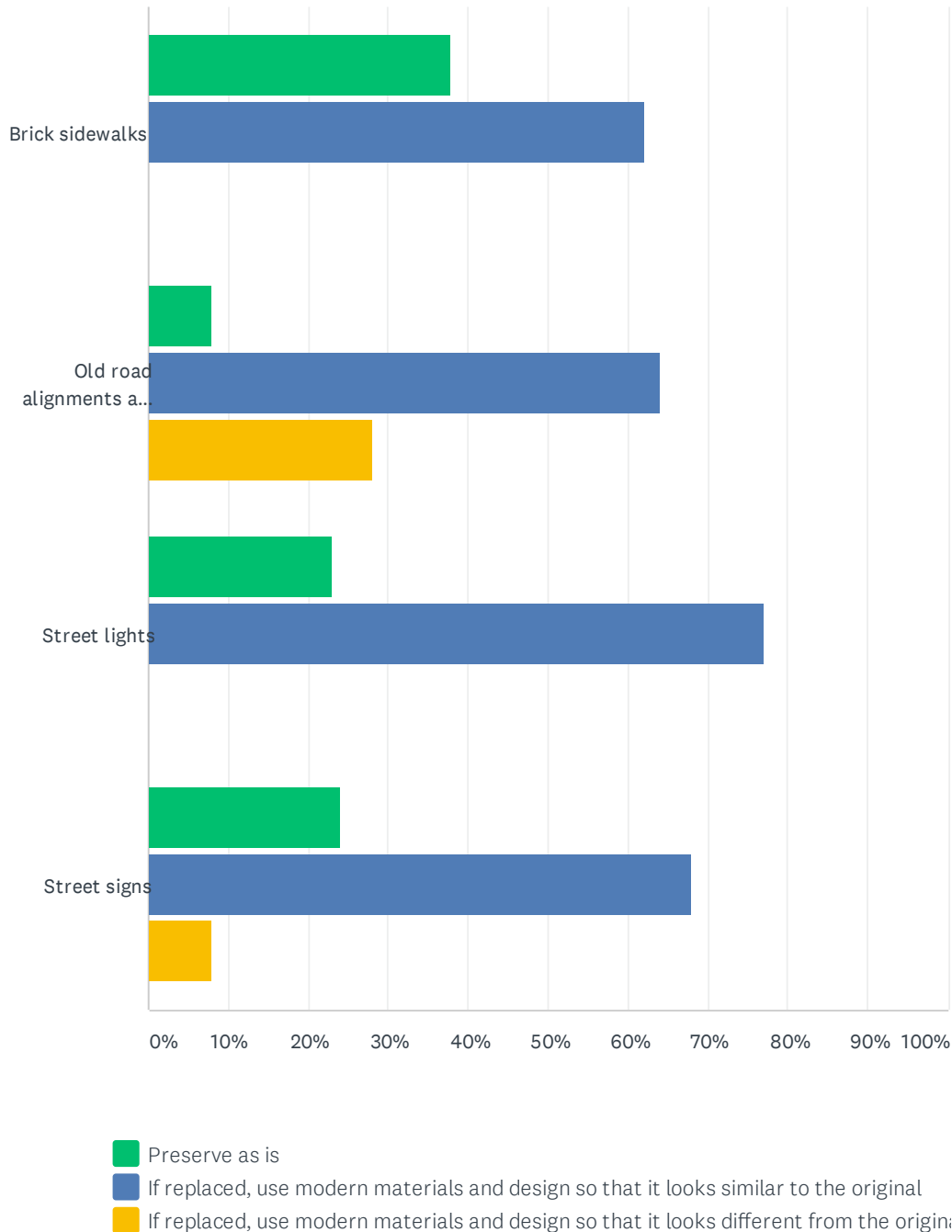
SurveyMonkey

	DAILY	AT LEAST ONCE A WEEK	AT LEAST ONCE A MONTH	RARELY	NEVER	TOTAL
I drive through Brookeville to run errands.	74.19% 23	22.58% 7	3.23% 1	0.00% 0	0.00% 0	31
I drive through Brookeville to get to other destinations.	70.00% 21	16.67% 5	3.33% 1	3.33% 1	6.67% 2	30
I use it to drive to work	60.00% 18	20.00% 6	0.00% 0	6.67% 2	13.33% 4	30
I walk or jog in Brookeville for exercise or enjoyment.	56.67% 17	33.33% 10	3.33% 1	6.67% 2	0.00% 0	30
I park a vehicle on or along a street in Brookeville.	34.48% 10	3.45% 1	3.45% 1	31.03% 9	27.59% 8	29
I visit friends or family who live in Brookeville.	27.59% 8	34.48% 10	17.24% 5	17.24% 5	3.45% 1	29
I use it to ride a bicycle or walk to work	24.14% 7	3.45% 1	17.24% 5	10.34% 3	44.83% 13	29
I ride a bicycle in Brookeville for exercise or enjoyment.	17.24% 5	20.69% 6	10.34% 3	24.14% 7	27.59% 8	29
I visit historical sites in Brookeville.	13.79% 4	13.79% 4	10.34% 3	58.62% 17	3.45% 1	29
I attend events in Brookeville.	3.45% 1	0.00% 0	44.83% 13	48.28% 14	3.45% 1	29
I attend church or religious services in Brookeville.	0.00% 0	6.90% 2	6.90% 2	20.69% 6	65.52% 19	29

#	OTHER (PLEASE SPECIFY)	DATE
1	Walk my dogs.	4/24/2021 9:22 PM
2	It would be nice to be able to pull out of my driveway safely, maybe lower speed limit, speed bumps along west side of Market street	4/22/2021 11:49 PM

Q5 If the streetscape is changed to increase pedestrian or bicycle safety or to slow traffic, how would you treat each of the following historic elements?

Answered: 31 Skipped: 1



	PRESERVE AS IS	IF REPLACED, USE MODERN MATERIALS AND DESIGN SO THAT IT LOOKS SIMILAR TO THE ORIGINAL	IF REPLACED, USE MODERN MATERIALS AND DESIGN SO THAT IT LOOKS DIFFERENT FROM THE ORIGINAL	TOTAL
Brick sidewalks	37.93% 11	62.07% 18	0.00% 0	29
Old road alignments and street remnants within public right-of-way	8.00% 2	64.00% 16	28.00% 7	25
Street lights	23.08% 6	76.92% 20	0.00% 0	26
Street signs	24.00% 6	68.00% 17	8.00% 2	25

#	OTHER (PLEASE SPECIFY)	DATE
1	I completely understand the need for a modernization for safety and use purposes but the town has charm from its history that makes it ones of the greatest places to live. If modernization needs to happen to allow for ease of walking/biking etc. then keeping the design would be ideal.	5/10/2021 10:27 AM
2	Please fix north street. the potholes are ridiculous	5/5/2021 7:50 AM
3	I don't have a strong opinion about alignments or signs.	5/4/2021 2:38 PM
4	Pop	4/25/2021 2:24 PM
5	Please narrow the roads to slow traffic and improve the existing gravel road with whatever will last the longest.	4/24/2021 9:22 PM
6	It won't let me choose If replaced, use modern materials similar to the original. That is what I choose for each.	4/24/2021 12:35 PM
7	Reduce or change the amount of street signage.	4/24/2021 10:43 AM
8	Preserve all historic elements	4/22/2021 11:49 PM
9	roads need to be reworked at market/high intersection to be safer	4/22/2021 6:17 PM
10	Street lights and sighns	4/22/2021 6:01 PM
11	I'd love to see a sidewalk all the way to Olney	4/22/2021 4:44 PM

Q6 What is the biggest challenge facing the town in preserving the features important to you?

Answered: 29 Skipped: 3

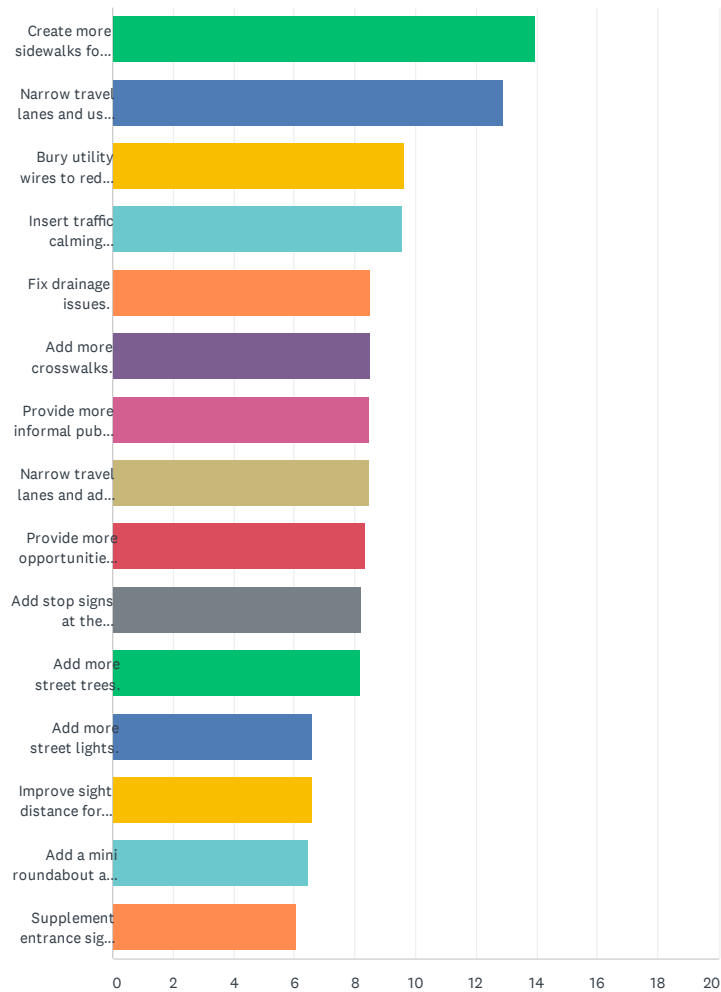
#	RESPONSES	DATE
1	Funding	5/11/2021 9:58 PM
2	Unknown. My guess is getting a consensus that changes are ok as long as they maintain the historical look, regardless if they use non-modern or modern materials.	5/11/2021 8:17 PM
3	maintaining the roads at a reasonable cost	5/5/2021 8:22 PM
4	I am not aware of any particular challenge. I do want to see the basic character of the town preserved and not lose the fact we are surrounded by trees. It would probably help improve the look of Brookeville is I could manage to plan a trees or shrubs in our yard that did not die immediately. :)	5/5/2021 3:00 PM
5	Budget and resistance to change	5/4/2021 8:06 PM
6	Money, unity of design	5/4/2021 7:57 PM
7	We need to preserve (and make new) paths and sidewalks to make the town very walk-able.	5/4/2021 5:51 PM
8	Improve the gravel roads	5/4/2021 5:22 PM
9	Differing opinions about what ought to be done.	5/4/2021 3:52 PM
10	Balancing modern needs and the historical aspect	5/4/2021 3:47 PM
11	Finding the money.	5/4/2021 3:10 PM
12	I'm honestly not sure. For me, walking safety is most important. (even more than historical preservation)	5/4/2021 2:38 PM
13	speeding traffic on the East Market street. Mostly traveling out of town onto Brighton Dam road, as they make the turn, cars immediately accelerate at a dangerously high rate of speed. Even when traffic comes from Brighton Dam road they are going way too fast into the town of Brookeville.	5/4/2021 12:32 PM
14	reducing traffic flow	5/4/2021 11:31 AM
15	Getting buy-in from residents to accept using more modern, safer, better performing, and more durable materials.	5/4/2021 11:22 AM
16	resurfacing roads and replacing curbs and sidewalks with historically appropriate materials	5/4/2021 10:52 AM
17	Funds	5/4/2021 10:40 AM
18	While Brookeville's history, embodied in its homes and landscape, is indispensable and unique, the Town feature most important to me is the ability for neighbors to continue to get to know each other in person. The biggest challenge to this is our inability to walk around Town easily and safely.	4/27/2021 5:59 PM
19	money - safety issues for the brick sidewalks	4/27/2021 11:45 AM
20	An appreciation and commitment to preserving Brookeville historic features and infrastructure consistent and respectful of those history.	4/25/2021 2:24 PM
21	Cost and historic requirements. The town needs to be upgraded and should use modern materials to do so. This can be accomplished while also maintaining the historic character in town.	4/24/2021 9:22 PM
22	Would love a continuous SAFE sidewalk connecting Historic Brookeville to Brookeville and then Brookeville to Olney. Having to cross back and forth by the academy and then the	4/24/2021 4:43 PM

graveyard can be harrowing, depending on the day,...

23	Funding, Safety	4/24/2021 12:35 PM
24	Improving the appearance and safety and blending with historical features.	4/24/2021 10:43 AM
25	Public opinion	4/23/2021 4:15 PM
26	Maintenance	4/22/2021 11:49 PM
27	Town needs some form of HOA to maintain properties and homes while preserving historic look. improvement is needed too, but should consider regulations to uphold investment.	4/22/2021 6:17 PM
28	It's not a matter of preserving the features important to me. The bypass should increase the quiet quaint nature of our little town I love. The roads (north south water and main) have been an issue since we moved in and need addressed. Improving the surfaces of the roads will make living and playing here better for all the residents.	4/22/2021 6:01 PM
29	intrusive neighbors who want to control what other people do on their own property	4/22/2021 4:44 PM

Q7 Which features of the streetscape would you like to change? Rank each feature starting with "1" as the highest priority, or select "N/A" if not applicable.

Answered: 31 Skipped: 1



Brookeville Questionnaire: Transportation, Pedestrian Networks & Streetscaping Elements

SurveyMonkey

	1	2	3	4	5	6	7	8	9	10	11	12	13
Create more sidewalks for safe pedestrian circulation throughout Town.	46.43% 13	25.00% 7	7.14% 2	7.14% 2	10.71% 3	0.00% 0	0.00% 0	0.00% 0	0.00% 0	0.00% 0	0.00% 0	0.00% 0	0.00% 0
Narrow travel lanes and use space to create new sidewalks, widen sidewalks, or add trees and plantings.	14.81% 4	29.63% 8	25.93% 7	14.81% 4	3.70% 1	0.00% 0	0.00% 0	0.00% 0	3.70% 1	3.70% 1	0.00% 0	0.00% 0	0.00% 0
Bury utility wires to reduce visual impact.	7.14% 2	7.14% 2	0.00% 0	17.86% 5	3.57% 1	10.71% 3	14.29% 4	7.14% 2	10.71% 3	3.57% 1	7.14% 2	0.00% 0	0.00% 0
Insert traffic calming measures to slow vehicular operating speeds.	14.81% 4	7.41% 2	7.41% 2	7.41% 2	3.70% 1	0.00% 0	3.70% 1	11.11% 3	7.41% 2	11.11% 3	7.41% 2	3.70% 1	0.00% 0
Fix drainage issues.	13.79% 4	6.90% 2	0.00% 0	6.90% 2	6.90% 2	10.34% 3	6.90% 2	3.45% 1	3.45% 1	3.45% 1	10.34% 3	10.34% 3	6.90% 2
Add more crosswalks.	0.00% 0	7.41% 2	7.41% 2	3.70% 1	7.41% 2	14.81% 4	7.41% 2	7.41% 2	7.41% 2	0.00% 0	3.70% 1	3.70% 1	7.41% 2
Provide more informal public green spaces (open spaces for gathering, benches, gardens, playgrounds, etc.).	3.57% 1	0.00% 0	3.57% 1	14.29% 4	17.86% 5	3.57% 1	7.14% 2	7.14% 2	10.71% 3	3.57% 1	0.00% 0	7.14% 2	7.14% 2
Narrow travel lanes and add on-road bicycle facilities.	3.70% 1	3.70% 1	14.81% 4	3.70% 1	7.41% 2	3.70% 1	7.41% 2	3.70% 1	7.41% 2	3.70% 1	3.70% 1	0.00% 0	7.41% 2
Provide more opportunities for natural surface trails in wooded areas around Town.	3.57% 1	0.00% 0	10.71% 3	0.00% 0	10.71% 3	21.43% 6	3.57% 1	3.57% 1	0.00% 0	10.71% 3	10.71% 3	0.00% 0	10.71% 3
Add stop signs at the intersection of High Street, Water Street, and Market Street.	3.57% 1	7.14% 2	7.14% 2	3.57% 1	10.71% 3	3.57% 1	7.14% 2	0.00% 0	7.14% 2	0.00% 0	3.57% 1	10.71% 3	0.00% 0
Add more street trees.	0.00% 0	0.00% 0	3.70% 1	7.41% 2	14.81% 4	3.70% 1	7.41% 2	22.22% 6	11.11% 3	11.11% 3	0.00% 0	3.70% 1	0.00% 0
Add more street lights.	0.00% 0	0.00% 0	11.11% 3	0.00% 0	0.00% 0	7.41% 2	11.11% 3	3.70% 1	3.70% 1	7.41% 2	0.00% 0	3.70% 1	0.00% 0
Improve sight distance for drivers and pedestrians at intersections and curved roadways where visibility is limited.	0.00% 0	0.00% 0	3.70% 1	7.41% 2	0.00% 0	3.70% 1	7.41% 2	7.41% 2	3.70% 1	11.11% 3	11.11% 3	3.70% 1	14.81% 4
Add a mini roundabout at the intersection of High Street, Water Street, and Market Street.	0.00% 0	10.34% 3	0.00% 0	6.90% 2	3.45% 1	0.00% 0	3.45% 1	6.90% 2	3.45% 1	10.34% 3	3.45% 1	6.90% 2	10.34% 3
Supplement entrance signs with attractive gateway landscapes.	0.00% 0	0.00% 0	0.00% 0	0.00% 0	0.00% 0	14.29% 4	10.71% 3	7.14% 2	10.71% 3	3.57% 1	7.14% 2	17.86% 5	0.00% 0

Q8 What is the biggest challenge facing the Town when it comes to the changes you would like to make?

Answered: 28 Skipped: 4

#	RESPONSES	DATE
1	Creating the space for new sidewalks	5/11/2021 9:58 PM
2	Timing	5/11/2021 8:17 PM
3	actually getting things done by consensus	5/5/2021 8:22 PM
4	I am worried about people wanting to go too far with changes. I am particularly concerned about people suggesting more speed bumps or worse, traffic cameras. The ones we have are already annoying enough. I'd prefer to see them gone. I have not noticed the traffic camera stopping speeding. One a Saturday night it is like a disco in our living room with the speed camera flashing so often. Hopefully the bypass will redirect the speeders. I don't want a bypass and more speed bumps. I want a bypass and no speed bumps. Making the junction of High, Market and Water Streets a three way stop should help slow down what traffic remains.	5/5/2021 3:00 PM
5	Fit the potholes on north street. It's no longer the 1700's. Use an appropriate material to handle today's vehicles and levels of traffic.	5/5/2021 7:50 AM
6	Budget and resistance to change	5/4/2021 8:06 PM
7	unsure	5/4/2021 7:57 PM
8	cost and consensus	5/4/2021 5:22 PM
9	Cost	5/4/2021 3:52 PM
10	Varied opinions	5/4/2021 3:47 PM
11	Lack of funds.	5/4/2021 3:10 PM
12	It is very challenging to cross the street in the morning or to walk without going into the cemetery.	5/4/2021 2:38 PM
13	\$	5/4/2021 11:31 AM
14	Getting residents to buy into changing the structure of the town and replacing historic materials with better modern materials.	5/4/2021 11:22 AM
15	lack of funding to repair/replace curbstones, sidewalks, road surfaces with historically appropriate designs and materials	5/4/2021 10:52 AM
16	Funds	5/4/2021 10:40 AM
17	Financing changes to the streetscape	4/27/2021 5:59 PM
18	money	4/27/2021 11:45 AM
19	Budgeting	4/25/2021 2:24 PM
20	Funding.	4/24/2021 9:22 PM
21	Space (width of road) to add safe sidewalks and bike safe lanes,...	4/24/2021 4:43 PM
22	Funding	4/24/2021 12:35 PM
23	Selecting the best contractor for this project. Cost will be an issue, but quality is paramount. Do anticipate for cost overruns. Applaud the Commissioners for addressing this necessary infrastructure project along with the resurfacing street projects in the Town.	4/24/2021 10:43 AM
24	money	4/23/2021 4:15 PM

Brookeville Questionnaire: Transportation, Pedestrian Networks & Streetscaping Elements

SurveyMonkey

25	West side of Market Street is not considered when decisions are made. Curve in road causes constant collisions and damage. Vibrations cause damage to homes directly on the street. No street lights, no sidewalk.	4/22/2021 11:49 PM
26	people not taking care of their properties and have no regard for the effect it has on the entire neighborhood, property values and appearance of streets and entire town!!	4/22/2021 6:17 PM
27	The residents who are challenging the efforts of the current commissioners to improve our town.	4/22/2021 6:01 PM
28	reaching group consensus	4/22/2021 4:44 PM

Q9 What improvement would make you more likely to use the streetscape area for walking?

Answered: 30 Skipped: 2

#	RESPONSES	DATE
1	Wider/longer sidewalks that are well lit.	5/11/2021 9:58 PM
2	The sidewalks need to be extended and on both sides of the road. Additionally they need to be widened. I feel uncomfortable walking with my children on these sidewalks considering how fast people travel on the roads.	5/11/2021 8:17 PM
3	paved roads and sidewalks	5/5/2021 8:22 PM
4	Well an improvement in my weight and energy level would be most likely to do that. After that, continuous sidewalks on High Street are number one. But if the city is going to take a slice of my lawn for a side walk, that should be a city maintenance area when it comes to snow removal.	5/5/2021 3:00 PM
5	I walk around town everyday with no problems.	5/5/2021 7:50 AM
6	Extend sidewalks narrow roads to make room for them	5/4/2021 8:06 PM
7	sidewalks, separation from road	5/4/2021 7:57 PM
8	More walkways away from the street. Continuous sidewalks on all of the main roads (High & Market Sts.).	5/4/2021 5:51 PM
9	sidewalks	5/4/2021 5:22 PM
10	Safe sidewalks!!	5/4/2021 3:52 PM
11	Better sidewalks that go throughout town	5/4/2021 3:47 PM
12	Slower Traffic.	5/4/2021 3:10 PM
13	I don't walk into the North Street direction because there aren't paths that connect. I'd love to be able to do a loop on sidewalks or other defined walking areas.	5/4/2021 2:38 PM
14	More accessibility to community sidewalks for freely and safely walking through the town.	5/4/2021 12:32 PM
15	reduced traffic flow and speed	5/4/2021 11:31 AM
16	Better sidewalks and trails to connect to Longwood and Gold Mine Rd.	5/4/2021 11:22 AM
17	more sidewalks	5/4/2021 10:52 AM
18	sidewalks	5/4/2021 10:40 AM
19	Narrowing the road would automatically slow traffic and allow more space for walking, making walking by the road much more pleasant.	4/27/2021 5:59 PM
20	more sidewalks on High St and West Market St	4/27/2021 11:45 AM
21	Safe walking surfaces and safe sidewalks.	4/25/2021 2:24 PM
22	Walkability.	4/24/2021 9:22 PM
23	Expanded continuous safe sidewalks.	4/24/2021 4:43 PM
24	Sidewalk on High St	4/24/2021 12:35 PM
25	Contiguous and safe sidewalks. Safely walk at least beyond the cemetery and Silo Falls. Sidewalk on Market Street is unsafe when wet or damp.	4/24/2021 10:43 AM
26	Sidewalks that are pedestrian-friendly. The brick sidewalks in the 200 block of Market St get slippery when damp	4/23/2021 4:15 PM

27	Continue sidewalk from 301-307 market through to 311, add crosswalk in front of academy, continue sidewalk in front of church to past cemetery, crosswalk to join to path in front of silo falls.	4/22/2021 11:49 PM
28	safer, more sidewalks , more connections	4/22/2021 6:17 PM
29	Hard surfaced roads and additional sidewalks.	4/22/2021 6:01 PM
30	sidewalks	4/22/2021 4:44 PM

Q10 What improvement would make you more likely to use the streets for bicycling?

Answered: 27 Skipped: 5

#	RESPONSES	DATE
1	Bicycle lanes.	5/11/2021 9:58 PM
2	They would need to repaved and traffic would have to be reduced significantly. There's also quite a bit of blind spots that would need to be addressed.	5/11/2021 8:17 PM
3	A bike lane	5/5/2021 8:22 PM
4	It seems an unlikely proposition at best. I just don't see taking up biking. But my kids might is there were continuous sidewalks.	5/5/2021 3:00 PM
5	I cycle thousands of miles annually, but it's no longer safe to ride in and out of Brookeville due to regional traffic patterns. Even with the bypass completed, I will seldom choose to cycle around Brookeville.	5/5/2021 7:50 AM
6	Slow traffic with narrower streets and on street parking	5/4/2021 8:06 PM
7	n/a	5/4/2021 7:57 PM
8	Better to build bike paths around town - away from the streets.	5/4/2021 5:51 PM
9	won't use	5/4/2021 5:22 PM
10	Traffic calming	5/4/2021 3:52 PM
11	A bike lane on Market Street and Georgia in town.	5/4/2021 3:47 PM
12	N/A	5/4/2021 3:10 PM
13	I don't bike! Others have mentioned that there isn't enough space for safe biking.	5/4/2021 2:38 PM
14	reduced traffic flow and speed	5/4/2021 11:31 AM
15	Bike lanes and less traffic.	5/4/2021 11:22 AM
16	N/A	5/4/2021 10:52 AM
17	Bike Lanes	5/4/2021 10:40 AM
18	Probably narrowing the streets. Also, if there were a safe way to bicycle up to the Brookeville Beer Farm (which would mean having some safe passage for bikes or pedestrians just north of the Bypass), I would bike through Town more.	4/27/2021 5:59 PM
19	NA	4/25/2021 2:24 PM
20	Narrower streets.	4/24/2021 9:22 PM
21	designated bike lanes,...	4/24/2021 4:43 PM
22	Pave the gravel roads.	4/24/2021 12:35 PM
23	If you calm traffic, it will make it better for bicycling. I do not see a need for a bike lane on the road	4/23/2021 4:15 PM
24	none	4/22/2021 11:49 PM
25	stop lights and safer crossing and more sidewalks	4/22/2021 6:17 PM
26	A new smooth surface.	4/22/2021 6:01 PM
27	bike lanes	4/22/2021 4:44 PM

Q11 Please provide additional comments and suggestions below. To provide comments or add photos on an interactive map of the town, please [click this link](#).

Answered: 11 Skipped: 21

#	RESPONSES	DATE
1	The historical looking street lights are not evenly distributed throughout the town. It would be a nice addition to areas that currently lack them.	5/11/2021 9:58 PM
2	Fix north street	5/5/2021 7:50 AM
3	On street parking provides a traditional village feel, while creating traffic calming and convenience. I highly recommend using this feature	5/4/2021 8:06 PM
4	None	5/4/2021 7:57 PM
5	The area in front of the cemetery is particularly concerning for me. Walking on the graves is not appropriate, but it is currently the only safe alternative. I've seen parents with strollers get stuck walking in the street, and that is concerning as well.	5/4/2021 2:38 PM
6	None.	5/4/2021 11:22 AM
7	Burying wires would improve the appearance of the town almost as much as replacing terrible curbs and road surfaces.	5/4/2021 10:52 AM
8	If we're focused on improvement, we should think about the infrastructure needs versus the aesthetic. For example, while town green spaces would be great, drainage issues are the most pressing. We should focus on making the town structurally sound first. If that is closely followed by aesthetic improvements, great. If there is a choice to be made, we should lean on the basic infrastructure needs first.	5/4/2021 10:40 AM
9	I would love to bring back the Post Office, or have a coffee shop, small market, or ice cream shop move in. I know that this would affect some people negatively (because of the parking and noise, etc.), but I think it would be an overall win for the town to have more gathering spaces that are worth going to on foot.	4/27/2021 5:59 PM
10	create a regulatory committee that creates and maintains community standards. then hold people accountable for standards of community.	4/22/2021 6:17 PM
11	N/A	4/22/2021 6:01 PM